

ATKINS

# **Baldoyle to Portmarnock Pedestrian & Cyclist Scheme**

EIA Screening Statement

Fingal County Council

**16 January 2018**

# Notice

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# 1. Introduction

## Background

- 1.1. Fingal County Council proposes to develop a pedestrian and cyclist scheme with a view to providing connecting facilities between the towns of Baldoyle and Portmarnock. This scheme will form part of the mitigation measures for the development of the Baldoyle-Stapolin and Portmarnock South Local Area Plans (LAPs).
- 1.2. The above-mentioned LAPs both require mitigation measures to avoid damage to the adjacent Baldoyle Bay, which is a Special Area of Conservation (SAC; site code 000199), Special Protection Area (SPA; site code 004016) and proposed Natural Heritage Area. SACs and SPAs are designated Natura 2000 sites under both Irish and European law. In order to avoid pedestrian and cycle traffic generated by the new developments as part of the LAPs impacting on these protected areas, a walking and cycling route is required between Baldoyle and Portmarnock. This will be located away from ecologically sensitive habitats and will be of sufficient quality to attract users to it.
- 1.3. As development has begun within the LAP lands and the population of the area will soon begin to increase significantly, this walking and cycling route is required as soon as possible to avoid negative impacts to Baldoyle Bay.
- 1.4. To achieve this objective, Fingal engaged Atkins to develop route options and to undertake the preliminary design work on the preferred route option. It is then intended to develop the detailed design of the project and progress it through construction.
- 1.5. In addition to the above, it is intended that the Baldoyle to Portmarnock Pedestrian and Cyclist scheme will form part of, and link to, a larger cycle network that will ultimately connect with similar facilities, including: -
  - The proposed Sutton to Malahide Pedestrian and Cyclist route, currently being developed, which will interface directly at either end of this scheme
  - A proposed cycle and pedestrian route which will run adjacent to the railway viaduct from Malahide to Donabate, and
  - The existing Sutton to Sandycove (S2S) cycle route running alongside the coast through Clontarf towards the city centre.
- 1.6. In this regard, the proposed Baldoyle to Portmarnock Pedestrian and Cyclist scheme will address a significant shortfall in the existing and proposed local / regional network for cyclists and walkers while also removing non-vehicular traffic from the SAC and SPA areas.
- 1.7. A Design Options Report was carried out by Atkins in May 2017, which identified a preferred route for the scheme. The preferred route was developed in more detail as part of the preliminary design.

## Purpose of this Report

- 1.8. This report has been prepared to support the Planning Application to An Bord Pleanála by describing the existing situation, the proposed scheme and its potential impact on the surrounding environment.
- 1.9. The purpose of this report is to determine whether the project requires an Environmental Impact Assessment (EIA). The proposed provision of a cycle and pedestrian route connecting facilities between the towns of Baldoyle and Portmarnock will be screened to generate a summarised overview of the potential impacts of the proposal on the receiving environment and in the context of the relevant statutory requirements. Should the likelihood of significant environmental effects

exist, mitigation measures and alternative solutions will be advised such that, as far as is practicable, there will be no significant environmental effect on the receiving environment as a result of this development.

- 1.10. A separate Natura Impact Statement (NIS) report which assesses impacts on designated Natura 2000 sites as a result of the proposed scheme has also been prepared by Atkins. The Natura Impact Statement has also been submitted as part of the technical documents in support of the planning application.

## Environmental Impact Assessment Screening

- 1.11. The determination of the environmental topics to be considered in this report, as well as the scope of each chapter, were informed by the following best practice guidance documents: -

- *'Guidelines on the Information to be contained in Environmental Impact Assessment Reports – Draft'*, EPA, 2017;
- *'Revised Guidelines on the Information to be contained in Environmental Impact Statements – Draft'*, EPA, 2015;
- *'Advice Notes For Preparing Environmental Impact Statements'*, EPA, 2015; and
- *'Environmental Impact Assessment (EIA) Guidance for Consent Authorities regarding Sub-threshold Development'* DoEHLG, 2003.

- 1.12. Relevant discipline specific guidance on impact assessment / best practice was also consulted.

## Stakeholder Consultation

- 1.13. Public consultation on the scheme was carried out by Fingal County Council (Fingal). The draft scheme was presented by Fingal as part of a public consultation evening for the Racecourse Park development on the 14<sup>th</sup> July 2017. This meeting was held in the Baldoyle Community Centre and was attended by approximately one hundred local residents. The scheme was very positively received by those in attendance with the main concerns raised being in relation to the upgrading of the Moyne Road/Coast Road junction and the potential impact of traffic signals on travel times for vehicular traffic between Baldoyle and Portmarnock.

- 1.14. Fingal staff have also met with residents along the route, who raised the following issues:

- Residents to the estuary side of the coastal road were concerned about the potential aesthetic and flooding impacts of the scheme.
- Residents at the Moyne Road junctions made recommendations for boundary treatment along their property and preventing vehicular access to the pathway. They also requested that the nearby bus stop be linked to the new pathway to increase the safety of accessing the bus stop.
- The residents to the landward side of the Coast Road between the Moyne Road junction and the Portmarnock roundabout were keen to see the scheme proceed and requested to have connections made from the proposed scheme to their properties by means of mown grass pathways.

The above suggestions and recommendations have all been addressed and incorporated in the final design of the scheme.

- 1.15. Consultations were also held with the following stakeholders:

### National Transport Authority (NTA)

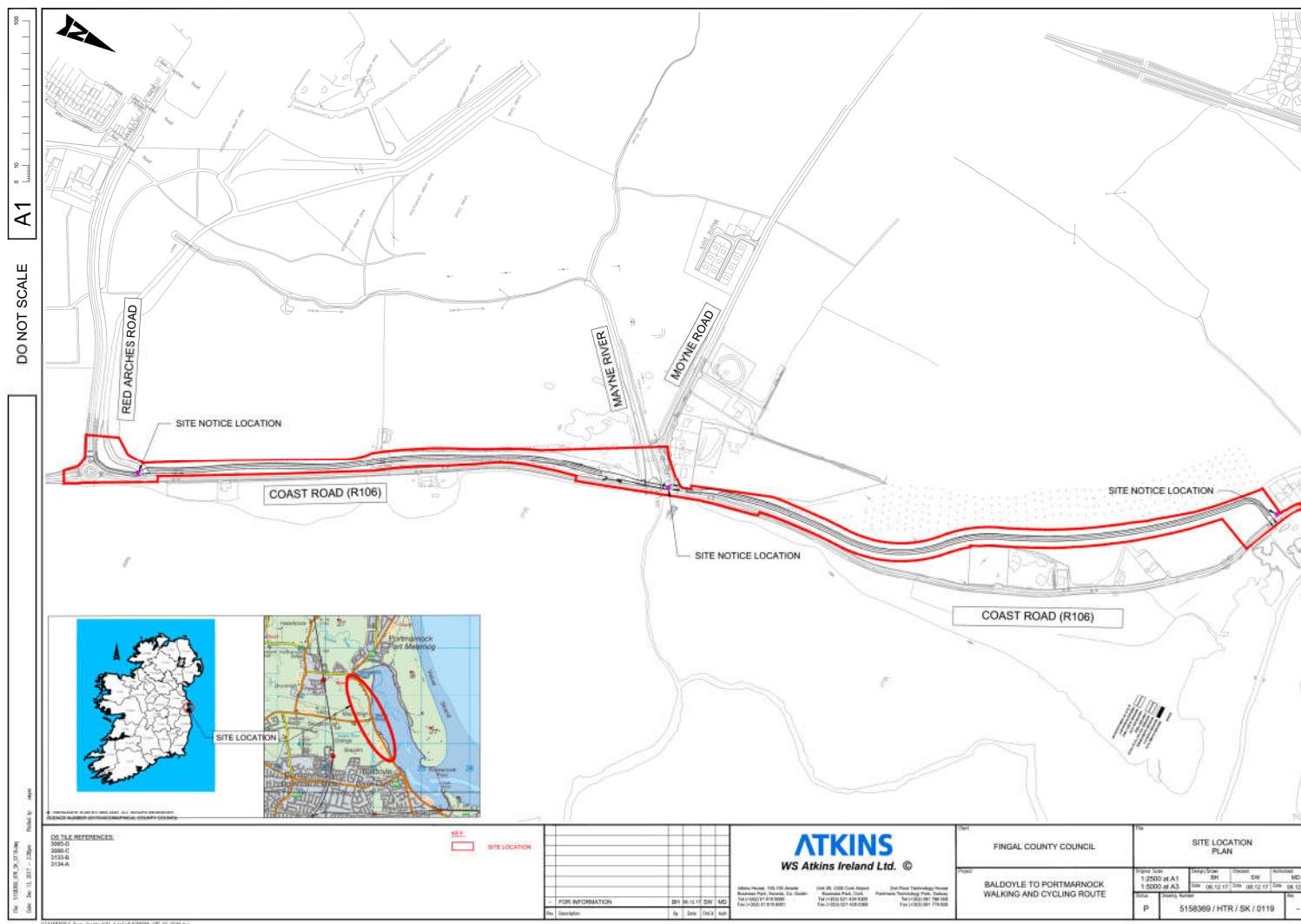
- 1.16. Several meetings were held between Fingal and the NTA to discuss pathway widths, design standards, links with other walking and cycling schemes and funding.

## National Parks and Wildlife Service

- 1.17. Fingal County Council officials met with the NPWS in July 2016 to discuss pathway width options and the potential impacts of the scheme on the Bald Doyle Bay Natura 2000 site.

## Utility and Service Providers

- 1.18. Utility companies and service providers were contacted to ascertain whether any development within the study area would have an impact on any of their existing infrastructure. This consultation was largely required to understand the engineering impacts of any particular route option upon existing services.



**Figure 1.1 Site Location and General Site Setting.**

## Description of the proposed scheme

### Route Selection Process

- 1.19. An Options Assessment was completed by Atkins in May 2017 to consider the context of the scheme in terms of key planning policies, to identify significant engineering / environmental constraints, to determine and evaluate route options, and ultimately to identify a preferred route. Various constraints / considerations were identified along the route as part of the assessment process. These were divided into two main categories: engineering and environmental considerations. Full details are presented in the Options Assessment Report (Atkins, 2017) and the Preliminary Design Report (Atkins, 2018), and are summarised below.

### Engineering Considerations

1. The route corridor is required to cross existing heavily trafficked roads at Red Arches Road, Moyne Road and the R106 Coast Road. There are currently no facilities for vulnerable road users at any of these locations.
2. There are several existing boundaries that the proposed scheme must take account of including a number of residential properties and the existing daa Non-Directional Beacon station.
3. There are several proposed boundaries that the proposed route must avoid including the Portmarnock South LAP lands, the proposed new pumping station being developed by Irish Water and a new wetlands area to be constructed as part of the developments in Portmarnock South.
4. There is an existing pumping station located at the junction of R106 Coast Road and R123 Moyne Road, resulting in a pinch point.
5. The route must cross the Mayne River, with the location of this crossing representing a significant constraint.

### Environmental Considerations

1. **Human Beings:** The proposed route will not add to operational sources of noise or air pollution but could assist in promoting more sustainable transport with associated reductions in emissions from vehicular traffic.
2. **Ecology:** The principal ecological constraints along the route are associated with the proximity of the Baldoyle Bay Special Protection Area (SPA) and Special Area of Conservation (SAC), which are Natura 2000 sites. Conservation interests of the site include coastal habitats and international and nationally important populations of birds. Key constraints to these sites include habitat loss and disturbance, both direct and indirect. It will be necessary for the route to cross the SAC for a short section south of the Moyne River. However, in line with the Local Area Plans the construction of the scheme will remove non-vehicular traffic from adjacent to the SAC/SPA areas for the majority of its length.
3. **Landscape and Visual Amenity:** The entirety of the proposed route passes through areas which are designated within the Fingal County Development Plan as Highly Sensitive Landscape. The route design will, therefore require the maintenance of the existing character of the landscape types through which it passes.

4. **Archaeology, Architectural and Cultural Heritage** There are 11 no. Record of Monuments and Places (RMP) sites within approximately 500m of the proposed route and 4 no. National Inventory of Architectural Heritage (NIAH) sites.
  5. **Soils and Geology:** The main constraints identified along the route are areas of Made Ground and Alluvial or Marine/Estuarine deposits.
- 1.20. Following the review of constraints, four route options were then developed for the scheme, and were assessed in line with current project appraisal guidelines using a multi-criterion analysis as follows; economy, safety, environment, accessibility & social inclusion and integration. Each route was assessed in a comparative manner, and ranked in order to identify the preferred route.

## Details of the Proposed Scheme

- 1.21. A full set of Design Drawings showing the preferred route accompany this planning application. A typical cross section of the preferred route is presented in Figure 1.2.

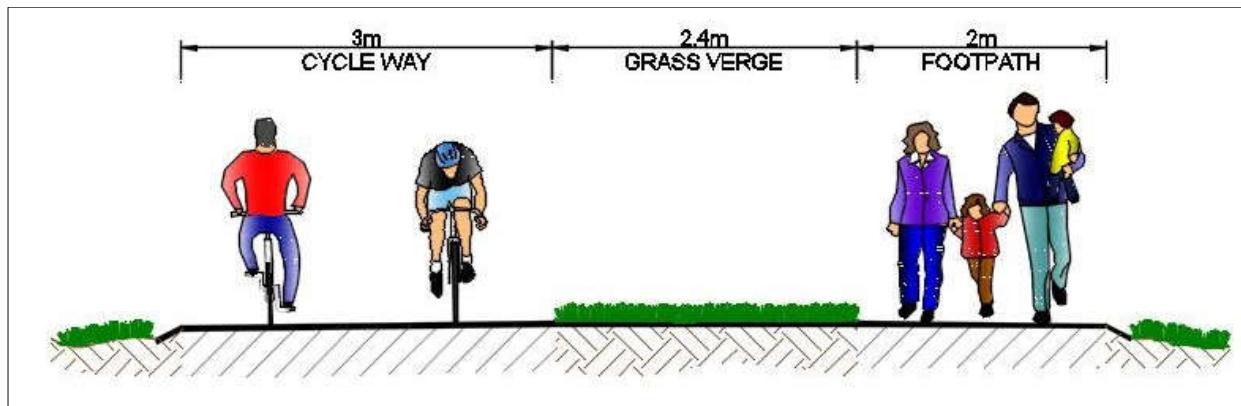


Figure 1.2 Preferred Route Typical Cross Section.

- 1.22. Several localised areas of shared surface at the northern and southern ends of the scheme will be required to facilitate access on existing roads. Crossing points are proposed at Red Arches Road, Mayne Road and Coast Road.
- 1.23. An additional area of shared surface is required across Mayne River due to the pinch point at the existing pumping station. A new, 4m wide bridge with anti-slip surfacing will be constructed over the Mayne River to accommodate pedestrian and cycle traffic only. The concrete bridge deck will be supported by steel piles on either side of the Mayne River; the depth of the piles will depend on encountered ground conditions, but are expected to be greater than 3m. The bridge handrails will comprise bespoke stainless steel pedestrian parapet posts with longitudinal tensioned wires and leaning rail.
- 1.24. Lighting will be installed along the length of the proposed scheme, based on the preliminary design following consultation between the design team, landscape and ecology specialists, and Fingal County Council. The preliminary lighting design comprises 6m lighting columns at 25m intervals along the proposed route. The poles will be located at the rear of the footpath (i.e. land side). The lights will dim by 50% between 11pm and 6am. The proposed crossing points and the junction at Mayne Road are already lit by streetlights; however, these lights may be upgraded during the proposed scheme. During the construction phase a site compound will be located at the north-eastern portion of the Red Arches / Coast Road junction, within the lands of the proposed route.

## 2. Methodology

### EIA Screening

- 2.1. Environmental Impact Assessment Screening was undertaken in line with Section 3.2 of the Revised Guidelines on the Information to be contained in Environmental Impact Assessment Reports (EPA, Draft August, 2017).
- 2.2. The first step is to determine if the proposed works represent a project as understood by the Directive. Such projects are defined in Article 4 of the EIA Directive and set out in Annexes I and II of the Directive (see Appendix A for details of these Annexes). However, it is not just a question of comparing the proposed works to the general description of project types; it may also be necessary to go further and consider the component parts of the proposed works and any processes arising from them.
- 2.3. The next step is to determine whether the project exceeds a specific threshold (as set out in the legislation). The only type of project to which thresholds do not apply are those considered to always be likely to have significant effects and therefore require an EIAR.
- 2.4. There are no exacting rules as to what constitutes “significant” in terms of environmental impacts. The responsibility is on Planning Authorities to carefully examine every aspect of a development in the context of Characterisation of the Project; Location of the project and Type & Characteristics of potential impacts (Table 2.3). It is generally not necessary to provide specialist studies or technical reports to complete this screening process, rather to investigate where further studies may be required, and where risks, if any, to the integrity of the receiving environment may lie.

### Legislative Context

- 2.5. The Environmental Impact Directive (85/337/EEC) was brought into force in 1985. Subsequent amendments were made with the following pieces of legislation - 97/11/EC, 2003/35/EC, 2009/31/EC and 2014/52/EU. The Directive was originally transposed into Irish Law by the European Communities (Environmental Impact Assessment) Regulations, 1989 (S.I. No. 349/1989). This amended the Local Government (Planning and Development Act) 1963, and introduced the requirement for an Environmental Impact Assessment in certain specified circumstances.
- 2.6. Annex I of the European Communities (Environmental Impact Assessment) Regulations lists the activities for which a Mandatory EIA is required. Annex II of the Regulations lists the activities for which the relevant Member State is to exercise discretion on whether or not an EIA is necessary. This decision must be made on a case-by-case basis. Annex III of the Regulations outlines the criteria which must be taken into consideration when a sub-threshold project is being examined for Environmental Impact. The full details of Annex I – III can be found at Appendix A of this report.
- 2.7. The most recent amendment to the Directive is focused on clarifying and simplifying the process of EIA. The screening criteria have been updated, and Member States have a mandate to simplify their assessment procedures. EIA reports are to be made more readily understandable to members of the general public.

### Mandatory Requirement for EIA

- 2.8. An EIA is required as a matter of course on specified large-scale projects which have a high likelihood of impacting on the receiving environment. These projects are listed in detail in the EIA

Directive, Annex I, (85/337/EU – amended 97/11/EC, 2003/35/EC, 2009/31/EC, 2014/52/EU), listed in Planning & Development Regulations, Schedule 5, Part 1 – Development for the purposes of Part 10. See Table 2.1 below for a list of activities requiring mandatory EIA (also see Appendix A).

**Table 2.1 Activities requiring Mandatory EIA.**

Process or Activity
Crude-oil refinery
Gasification & Liquifaction
Nuclear Power
Radioactive Waste Storage & Disposal
Melting of cast-iron or steel
Extraction or processing of asbestos, or products containing asbestos.
Integrated chemical installation
Railway line
Aerodrome runway
Trading port or inland waterway
Disposal of hazardous waste

- 2.9. The proposed Baldoyle to Portmarnock Pedestrian & Cyclist Scheme does not fall within the above list of activities requiring mandatory EIA.
- 2.10. Accordingly, this project has been further considered under the relevant list of activities which warrant discretionary consideration for the requirement of an EIA.

## Requirement for Member State Discretionary Decision on EIA

- 2.12. Each EU Member State has discretionary consideration for the requirement of an EIA in relation to the various processes and activities listed in Table 2.2 (listed in Planning & Development Regulations, Schedule 5, Part 2 – Development for the purposes of Part 10) (see also Appendix A).

**Table 2.2 Activities requiring consideration for EIA.**

Process or Activity
Intensive Agriculture, including salmon farming and land reclamation
Extractive industries, including peat extraction, associated processes and geothermal drilling.
Energy industry
Processing of metals
Manufacture of glass
Chemical Industry
Food industry
Textile, leather, wood and paper industries
Rubber industry
Infrastructure projects
Other projects
All modifications to specified developments

- 2.13. The proposed Baldoyle to Portmarnock Pedestrian & Cyclist Scheme does not fall within the above list of activities which would warrant discretionary consideration for the requirement of an EIA. Hence the preparation of an EIA for this project is not required under Schedule 5, Part 1 or Part 2 of the relevant Planning & Development Regulations.

## Criteria for Evaluation of Processes and Activities

- 2.15. Criteria to evaluate whether impacts are significant on the environment arising from developments are listed in Table 2.3 (listed in Planning & Development Regulations, 2001 Schedule 7, *Criteria for determining whether a development would or would not be likely to have significant effects on the environment*).

**Table 2.3 Criteria for determining whether a development would or would not be likely to have significant effects on the environment.**

Criteria	Summary of Level of Detail	Phase
Characterisation of the project	<p>The characteristics of proposed development, in particular: -</p> <ul style="list-style-type: none"> <li>➤ the size of the proposed development,</li> <li>➤ the cumulation with other proposed development,</li> <li>➤ the nature of any associated demolition works, – the use of natural resources,</li> <li>➤ the production of waste,</li> <li>➤ pollution and nuisances,</li> <li>➤ the risk of accidents, having regard to substances or technologies used.</li> </ul>	Construction Operational
Location of the project	<p>The existing land use: -</p> <ul style="list-style-type: none"> <li>➤ the relative abundance, quality and regenerative capacity of natural resources in the area,</li> <li>➤ the absorption capacity of the natural environment, paying particular attention to the following areas: -</li> <ul style="list-style-type: none"> <li>(a) wetlands,</li> <li>(b) coastal zones,</li> <li>(c) mountain and forest areas,</li> <li>(d) nature reserves and parks, Comment [i547]: Substituted by article 8 of S.I. No. 235/2008 – Planning and Development Regulations 2008 393</li> <li>(e) areas classified or protected under legislation, including special protection areas designated pursuant to Directives 79/409/EEC and 92/43/EEC,</li> <li>(f) areas in which the environmental quality standards laid down in legislation of the EU have already been exceeded,</li> <li>(g) densely populated areas,</li> <li>(h) landscapes of historical, cultural or archaeological significance.</li> </ul> </ul>	
Type and characteristics of the potential impact	<p>Extent of the impact having particular regard to: -</p> <ul style="list-style-type: none"> <li>➤ the extent of the impact (geographical area and size of the affected population),</li> <li>➤ the transfrontier nature of the impact,</li> <li>➤ the magnitude and complexity of the impact,</li> <li>➤ the probability of the impact,</li> <li>➤ the duration, frequency and reversibility of the impact.</li> </ul>	Construction Operational

## 3. Characterisation of the proposed development

### The size of the proposed development

- 3.1. Fingal County proposes to develop a comprehensive cycle and pedestrian route with a view to providing connecting facilities between the towns of Baldoyle and Portmarnock. This scheme will form part of the mitigation measures for the development of the Baldoyle-Stapolin and Portmarnock South Local Area Plans (LAPs). The extent of the scheme is illustrated in Figure 1.1; see also Plate 3.1 and 3.2 for a before and after view of the scheme north of Baldoyle. The lands proposed for development are already fenced off from adjoining lands (see Plate 3.1). The median strip of grassland is also to be used for biodiversity gain.



Plate 3.1 Land corridor along which the proposed Pedestrian & Cyclist Scheme will run - Before.



Plate 3.2. Land corridor along which the proposed Pedestrian & Cyclist Scheme will run – After.

## The cumulative impact with other proposed developments

- 3.2. As noted, this scheme will form part of the mitigation measures for the development of the Baldoyle-Stapolin and Portmarnock South Local Area Plans (LAPs). In order to avoid pedestrian and cycle traffic generated by the new developments as part of the LAPs impacting on protected areas in Baldoyle Bay, a walking and cycling route is required between Baldoyle and Portmarnock. This will be located away from the shoreline and be of sufficient quality to attract users to it and away from the sensitive habitat areas in Baldoyle Bay.
- 3.3. As development has begun within the LAP lands and the population of the area will soon begin to increase significantly, this walking and cycling route is required as soon as possible to avoid negative impacts to Baldoyle Bay.
- 3.4. In addition to the above, it is intended that the Baldoyle to Portmarnock Walking and Cycling Scheme will form part of, and link to, a larger cycle network that will ultimately connect with similar facilities, including: -
- The proposed Sutton to Malahide Cycling and Pedestrian route, currently being developed, which will interface directly at either end of this scheme;
  - A proposed cycle and pedestrian route which will run adjacent to the railway viaduct from Malahide to Donabate; and,
  - The existing Sutton to Sandycove (S2S) cycle route running alongside the coast through Clontarf towards the city centre.

- 3.5. In this regard, the proposed Baldoyle to Portmarnock Walking and Cycling Scheme will comprehensively address a significant shortfall in the existing and proposed local / regional network for cyclists and walkers while also removing non-vehicular traffic from the SAC and SPA areas.

## The use of natural resources

- 3.6. Other than construction materials, the proposed development will not require the use of any natural resources as the construction work will be limited to the provision of the Pedestrian & Cyclist Scheme on development lands and grassland between Portmarnock and Baldoyle. The proposed development corridor is located within the Racecourse Park; it does not host any identified viable aggregate reserves.

## The production of waste

- 3.7. The construction phase of the development may generate waste such as plastic wrapping or wooden pallets, but the waste will be removed off – site to the site compound where it will be stored before it is recycled or disposed of at a licensed waste facility.
- 3.8. The typical excavation depth will be approximately 0.5m along the majority of the proposed scheme. Deeper excavations may be required during the installation of underground utilities; however, such excavations will be localised. Waste soils and construction waste generation will be minimised during the proposed development, in accordance with Objective WM05 of the Fingal Development Plan 2017-2023 (FCC, 2017) as follows: -

*'Prevent and minimise the generation of waste in accordance with the Eastern Midlands Region Waste Management Plan 2015 -2021 (or any subsequent plans).'*

- 3.9. All excavated soils will be reused locally where possible, in accordance with Objective WM07 of the Fingal Development Plan 2017-2023 (FCC, 2017) as follows: -

*'Promote the increased re-use of waste in accordance with the Eastern Midlands Region Waste Management Plan 2015 -2021 (or any subsequent plan).'*

- 3.10. Any excess waste soils will be disposed of in accordance with all relevant Waste Management Legislation including the Waste Management Acts, 1996 to 2011, and any subsequent amendments.
- 3.11. All other construction waste will be segregated and removed from the site for disposal or recycling, in accordance with all relevant Waste Management Legislation.
- 3.12. Taking account of the waste management controls which will apply to the construction and the operational phases of the proposed scheme, waste generated during the proposed works will not have a significant effect on the receiving environment.

## Residues and Emissions during operation

- 3.13. The proposed Pedestrian & Cyclist Scheme is non-vehicular. There will be no emissions to air during operation.
- 3.14. Drainage of the Pedestrian & Cyclist Scheme will be by over-the-edge drainage and infiltration. There will be no discharge points to neighboring Natura 2000 sites or watercourses, such as the River Mayne.

## **The risk of accidents, having regard to substances and techniques used**

- 3.15. The risk of accidents associated with the construction phase is low given to the relatively small scale of the works required. The risks associated with such work are those that are commonly associated with working with machinery and assuming standard health and safety procedures are adopted, implemented and complied with the risk of accidents is low.
- 3.16. The provision of walk / cycleway, which is separated from the R106 Coast Road / Moyne Road, as well as a footbridge crossing of the River Mayne, will improve the safety for both pedestrians and motorists travelling between Baldoyle and Portmarnock.

## 4. Location of Proposed Development

- 4.1. Fingal County proposes to develop a comprehensive cycle and pedestrian route with a view to providing connecting facilities between the towns of Baldoyle and Portmarnock. This scheme will form part of the mitigation measures for the development of the Baldoyle-Stapolin and Portmarnock South Local Area Plans (LAPs). The extent of the scheme is illustrated in Figure 1.1; see also Plate 3.1 and 3.2 for a before and after view of the scheme north of Baldoyle.

### Current land-use

- 4.2. The scheme is located within the existing Racecourse Park and runs along the western side of the R106 Coast Road. Currently a corridor is fenced off as indicated on Plate 3.1; this is an area of mixed grassland. Lands to the west are part of ongoing habitat creation and habitat enhancement works being undertaken by Fingal County Council. To the east the site is bordered by the R106 Coast Road, which runs alongside Baldoyle Bay SAC / SPA.

### Abundance, quality and regenerative capacity of natural resources in the area

- 4.3. Currently a corridor is fenced off as indicated on Plate 3.1; this is presently an area of mixed grassland. Lands to the west are part of ongoing habitat creation and habitat enhancement works being undertaken by Fingal County Council. Other than the footprint illustrated in drawings in Appendix A (see also Plate 3.2), natural resources in the area are not required to facilitate the provision of the Pedestrian & Cyclist Scheme and footbridge over the River Mayne.

### Land

- 4.4. Currently a corridor is fenced off as indicated on Plate 3.1; this is presently an area of mixed grassland. Lands to the west are part of ongoing habitat creation and habitat enhancement works being undertaken by Fingal County Council. Other than the footprint illustrated in drawings in Appendix A (see also Plate 3.2), further land take in the area is not required to facilitate the provision of the Pedestrian & Cyclist Scheme and footbridge over the River Mayne.

### Soil

- 4.5. The typical excavation depth will be approximately 0.5m along the majority of the proposed scheme. Deeper excavations may be required during the installation of underground utilities; however, such excavations will be localised. Piling works will be required during the proposed bridge construction; however, these works will be limited and carefully managed to ensure no adverse soil impacts occur. Waste soils and construction waste generation will be minimised during the proposed development, in accordance with Objective WM05 of the Fingal Development Plan 2017-2023 (FCC, 2017). All excavated soils will be reused locally where possible, in accordance with Objective WM07 of the Fingal Development Plan 2017-2023 (FCC, 2017).

## Biodiversity

- 4.6. The proposed Baldoyle to Portmarnock Pedestrian & Cyclist Scheme is located along the eastern side of Baldoyle Bay. As a consequence it runs close to and partially within designated sites associated with Baldoyle Bay; namely Baldoyle Bay Special Area of Conservation (000199) and Baldoyle Bay Special Protection Areas for birds (004016). The Pedestrian & Cyclist Scheme lies entirely outside the SPA; a section of the proposed path is within the SAC. For assessment of ecological impacts refer to the Natura Impact Statement, which addresses impacts in and proximal to Natura 2000 sites.
- 4.7. Those areas which are located outside of Natura 2000 sites currently include areas of public infrastructure, amenity grassland and intensive agricultural lands which would be evaluated as being of *local importance (lower value)* (NRA, 2009)<sup>1</sup>. Loss / change to such habitats would not represent a significant ecological impact. Much of the path would impact locally important dry calcareous grassland (GS1)<sup>2</sup>; however, the area in which the proposed path would be located is already subject to trampling / walking pressure. As noted above, ongoing works within the Racecourse Park and proposals to manage grassland immediately adjoining the route to create species rich semi-natural grassland will enhance local biodiversity and associated conservation value.
- 4.8. With respect to agricultural lands at Mayne Bridge, the scale of impact would not significantly impact upon the use of these habitats by farmland bird species such as Yellowhammer, Tree Sparrow, Stock Dove etc., which do occur in notable numbers in this part of Fingal; especially when viewed against management measures being implemented within the Racecourse Park by Fingal County Council. It was also concluded that the proposed project would not negatively impact upon the conservation objectives of Baldoyle Bay SPA. The higher ground to the west of the path screens it from habitat creation measures to the west; it is also fenced to prevent access to these areas; thus preventing the risk of disturbance to e.g. ground nesting birds.
- 4.9. It is not proposed to remove hedges or trees. Recent surveys in the wetlands further west of the proposed route found no evidence of Borrer's saltmarsh grass (*Puccinellia fasciculata*) and Meadow barley (*Hordeum secalinum*); though as noted habitat management proposals on neighbouring lands are aimed and creating habitat for these species; seeds of which are likely to still occur in the seed bank. The design of the final bridge solution at Mayne River will be cognisant of any such plans.
- 4.10. The proposed route was not found to be of significance for bats. Mitigation measure are proposed to prevent negative impacts arising from lighting and ongoing habitat creation on adjoining lands will improve the conservation value of the general areas for bats.
- 4.11. An assessment of the potential impacts of the development of the coastal path and cycleway on wintering and migratory birds associated with Baldoyle Bay and on the qualifying interests for the designated Special Protection Area has been completed by Natura Environmental Consultants (2017), on behalf of FCC. Based on the findings of this report the following conclusions were made:
- The proposed route is very suitable for a coastal path and cycleway with expansive views of the estuarine habitats and coastal landscapes. The majority of the route is sufficiently far

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<sup>1</sup> NRA (2009). *Guidelines for Assessment of Ecological Impacts on national road schemes*.

<sup>2</sup> Fitzgerald (2017) notes that the majority of grassland in the study corridor corresponds to Fossitt's (2000) *dry calcareous and neutral grassland* (GS1). Furthermore, while the vegetation that characterised it has an affinity with the Annex I habitat 6210 *semi-natural dry grasslands and scrubland facies on calcareous substrates (Festuco-Brometalia)*, the dry grassland on site does not correspond to the EU Annex I habitat. That said, due to neglect the grassland has developed into a more species rich / semi-natural state over recent decades and is of local conservation value. As per the recommendation of Fitzgerald (2017) the project will attempt to retain as much of this habitat as practical; and further enhance where possible.

from the tidal area and is separated from it by a busy road so that it will not cause significant disturbance to non-breeding waterbirds.

- No screening from an ecological perspective is required on any part of the proposed route as there is no risk of significant disturbance to waterbirds either on the estuary or on the lands to the west of the route. Most of the western side of the route is already fenced so this should prevent uncontrolled dogs causing disturbance should certain areas to the west of the route be used in future by foraging or roosting waterbirds.
- The proposed route will not cause any significant impact on the Baldoyle Bay SPA or the Baldoyle Bay SAC. The path and cycleway will not interfere with the distribution or density of the species or habitats that are qualifying interests of these Natura 2000 sites. There is no likelihood that there will be any adverse effects on the favourable conservation condition of these sites.
- It is recommended that signage is erected at the three locations where the route is closest to the shoreline of Baldoyle Bay. The signs should provide information on the birds of the area and explain that dogs should be kept on the lead while on the pathway.

## Resilience of the receiving environment

- 4.12. The proposed Pedestrian & Cyclist Scheme and footbridge over the Mayne River are located back from the R106 Coast Road and are largely screened from lands to the west.
- 4.13. The entirety of the proposed route passes through areas which are designated within the Fingal County Development Plan as Highly Sensitive Landscape. The route has therefore been designed to maintain the existing character of the landscape types through which it passes; see Plate 3.1 and 3.2 for a comparison of the existing and proposed layout north of Red Arches Road. The only proposed pieces of infrastructure are the proposed lighting poles and the footbridge over the Mayne River. The proposed Pedestrian & Cyclist Scheme and footbridge over the Mayne River should be easily absorbed in the existing setting.
- 4.14. The bridge will have a span in the order of 30 meters. To minimize the construction impacts it will be formed using pre-cast beams which will be lifted into place using a mobile crane.



**Plate 4.1 Existing road bridge over the River Mayne.**



**Plate 4.2 River Mayne road bridge (left); proposed pedestrian bridge (right).**



Plate 4.3 R106 Coast Road (south of Portmarnock Bridge).



Plate 4.4 Proposed crossing point on the R106 Coast Road (south of Portmarnock Bridge).

## Presence of any designations

- 4.15. The proposed Baldoyle to Portmarnock Pedestrian & Cyclist Scheme is located along the eastern side of Baldoyle Bay. As a consequence it runs close to and partially within designated sites associated with Baldoyle Bay; namely Baldoyle Bay Special Area of Conservation (000199) and Baldoyle Bay Special Protection Areas for birds (004016). The footprint for the pathway land take from the SAC is approximately 455m in length with a width of less than 10m. For assessment of ecological impacts refer to the Natura Impact Statement, which addresses impacts in and proximal to Natura 2000 sites. It is considered that the land take in the SAC will not result in significant impacts to the SAC as presented in the Natura Impact Statement.
- 4.16. There are no protected built structures within the proposed development corridor.

## Sensitivity of the receiving environment

- 4.17. The proposed Baldoyle to Portmarnock Pedestrian & Cyclist Scheme is located along the eastern side of Baldoyle Bay. As a consequence it runs close to and partially within designated sites associated with Baldoyle Bay; namely Baldoyle Bay Special Area of Conservation (000199) and Baldoyle Bay Special Protection Areas for birds (004016). The footprint for the pathway land take from the SAC is approximately 455m in length with a width of less than 10m. For assessment of ecological impacts refer to the Natura Impact Statement, which addresses impacts in and proximal to Natura 2000 sites. It is considered that the land take in the SAC will not result in significant impacts to the SAC as presented in the Natura Impact Statement.
- 4.18. There are no protected structures within the route corridor.
- 4.19. The entirety of the proposed route passes through areas which are designated within the Fingal County Development Plan as Highly Sensitive Landscape.
- 4.20. The proposed route passes close to 3 no. residential properties along R106 Coast Road; to the front of 1 no. after crossing Moyne Road and behind 2 no. residential properties on the approach to Portmarnock Bridge. The route will be screened as required from these properties.
- 4.21. There is a history of flooding at two areas in the vicinity of the route; the existing Mayne Bridge and along the coastal road, R106 (OPW, 2017). It is not envisaged that the proposed cycle and pedestrian infrastructure would have any adverse impact on flooding along the route. The proposed development poses no significant potential flood risk to the surrounding lands or properties. The route of the proposed scheme includes an area identified to be potentially at risk from fluvial flooding (The Mayne Bridge). However, this is a very localised area and the scheme will be designed to take account of this potential risk. No potential flood risk has been identified along the remainder of the scheme.
- 4.22. The localised change in land surface and the improved drainage systems may result in a minor increase in rainfall run-off rates. However, such increases will be minor relative to existing conditions, and are unlikely to have a significant impact on the existing hydrological regime along the route of the proposed scheme. Therefore, it is not envisaged that the proposed development will pose any significant potential flood risk to the surrounding lands, properties or the surrounding road network (R123, R106).
- 4.23. Furthermore, in the unlikely event that flooding would occur at the Mayne Bridge, the scheme falls under the following general land-use category '*amenity open space, outdoor sports and recreation*'. As such, the proposed scheme, is considered to be a '*water compatible development*' in accordance with relevant flood risk assessment management guidance (OPW, 2009), and is therefore considered to be a suitable development for this area.

## 5. Type and Characteristics of Potential Impacts

- 5.1. The proposed project involves the construction of a pedestrian route and cycleway from Baldoyle to Portmarnock; a footbridge over the Mayne River and associated lighting.

### Extent of the impact (geographical area and size of the affected population)

- 5.2. The extents of potential impacts are limited to the footprint (approximately 18,000m<sup>2</sup>) of the proposed development (See drawings included in Appendix A).
- 5.3. Provision of the proposed Pedestrian & Cyclist Scheme will be a positive impact as it will provide safer conditions for pedestrians and motorists negotiating the route between Baldoyle and Portmarnock.

### Transfrontier nature of the impact

- 5.4. There is no potential for transfrontier impacts as a result of the proposed Pedestrian & Cyclist Scheme.

### Magnitude and complexity of the impact

- 5.5. Significant effects on the receiving environment are not anticipated as a result of the provision of Pedestrian & Cyclist Scheme.

### Probability of the impact

- 5.6. The probability of impacts on the receiving environment, such as pollution of receiving waters, is considered to be extremely low. Localised impacts such dust generation during construction are probable.

### Duration, frequency and reversibility of the impact

- 5.7. The probability of impacts on the receiving environment, such as pollution of receiving waters, is considered to be extremely low.

## 6. Screening Conclusion

- 6.1. Given the scale and nature of the project the probability of impacts on the receiving environment is considered to be extremely low, as presented in Table 6.1 below. No significant environmental impacts are anticipated.
- 6.2. The preparation of an EIA, is therefore not required for the proposed Baldoyle to Portmarnock Pedestrian & Cyclist Scheme, under the Planning and Development Act 2000, as amended and incorporating the Planning and Development Regulations 2001, as amended.
- 6.3. However, it is recommended that a non-statutory Environmental Report be prepared to accompany the application to An Bord Pleanála in order to outline the environmental baseline and predict the associated (albeit extremely low) impacts of the proposed scheme along with proposed mitigation measures.

**Table 6.1 EIA Checklist.**

Checklist	
Will there be a large change in environmental conditions?	No.
Will new features be out-of-scale with the existing environment?	No. The entirety of the proposed route passes through areas which are designated within the Fingal County Development Plan as Highly Sensitive Landscape. The route has therefore been designed to maintain the existing character of the landscape types through which it passes; see e.g. Plate 3.1 and 3.2 for a comparison of the existing and proposed layout north of Red Arches Road.
Will the effect be particularly complex?	No
Will the effect extend over a large area?	No The Pedestrian & Cyclist Scheme runs between Baldoyle and Portmarnock as illustrated on Figure 1.1.
Will there be any potential for trans-frontier impact?	No
Will many people be affected?	No; impacts should be positive overall due to a creation of a dedicated pedestrian / cycle route separated from the R106 Coast Road. The proposed route passes close to 3 no. residential properties along R106 Coast Road; to the front of 1 no. after crossing Moyne Road and behind 2 no. properties. The route will be screened as required from these properties.
Will many receptors of other types (fauna and flora, businesses, facilities) be affected?	No (refer also to accompanying Environmental Report and NIS)
Will valuable or scarce features or resources be affected?	No
Is there a risk that environmental standards will be breached?	No
Is there a risk that protected sites, areas, features will be affected?	No, the sensitivities of the SAC have been considered in the scheme design and the associated mitigation measures (refer also to the accompanying NIS)
Is there a high probability of the effect occurring?	No
Will the effect continue for a long time?	No

Will the effect be permanent rather than temporary?	n.a.
Will the impact be continuous rather than intermittent?	n.a.
If it is intermittent will it be frequent rather than rare?	n.a.
Will the impact be irreversible?	n.a.
Will it be difficult to avoid, or reduce or repair or compensate for the effect?	No

## 7. References

- Department of the Environment, Community & Local Government. (2013), *Guidelines for Planning Authorities and An Bord Pleanála on carrying out Environmental Impact Assessment*.
- Environmental Protection Agency (EPA), 2017. 'Revised Guidelines on the Information to be contained in Environmental Impact Assessment Reports – Draft'
- Environmental Resources Management (2001) *Guidance on EIA Screening*. Published by the European Commission
- Department of the Environment, Heritage and Local Government (2003) *Guidance for Consent Authorities regarding sub-threshold Development*. Published by the Stationery Office.
- European Commission, (2015) *Environmental Impact Assessment – EIA, Overview, Legal context*.
- European Council Directive (EC) 97/11/EC of 3 March 1997 amending Directive 85/337/EEC on the assessment of the effects of certain public and private projects on the environment.
- European Council Directive (EU) 2014/52/EU of 16 April 2014 amending Directive 2011/92/EU on the assessment of the effects of certain public and private projects on the environment.
- Fingal County Council (2017). *Fingal County Development Plan, 2017-2023*.
- Fitzgerald, A. (2017). Vegetation Assessment for a proposed walking and cycling route at Baldoyle Racecourse Park, Co. Dublin. Report for Fingal County Council.
- Fossitt, J. (2000). *A Guide to habitats in Ireland*. The Heritage Council.
- European Council Directive (EC) 85/337/EU of 1985 on Environmental Impact Directive.
- Office of Public Works (2009). '*The Planning System and Flood Risk Management; Guidelines for Planning Authorities*'.
- Office of Public Works (2017). OPW National Flood Hazard Mapping Web Site. Available at: - <http://www.floodmaps.ie/>. Consulted August 2017.
- Natura Environmental Consultants (2017). Baldoyle to Portmarnock Coastal Path and Cycleway. Assessment of potential impacts on birds. Report for Fingal County Council.
- NRA (2009). *Guidelines for Assessment of Ecological Impacts on national road schemes*. Published by National Roads Authority.

# Appendices



## Appendix A. Annexes of the Regulations

## A.1. ANNEX I - Projects Subject to Article 4(1) of the EIA Directive

- Crude-oil refineries (excluding undertakings manufacturing only lubricants from crude oil) and installations for the gasification and liquefaction of 500 tonnes or more of coal or bituminous shale per day.
- Thermal power stations and other combustion installations with a heat output of 300 megawatts or more and nuclear power stations and other nuclear reactors (except research installations for the production and conversion of fissionable and fertile materials, whose maximum power does not exceed 1 kilowatt continuous thermal load).
- Installations solely designed for the permanent storage or final disposal of radioactive waste.
- Integrated works for the initial melting of cast-iron and steel.
- Installations for the extraction of asbestos and for the processing and transformation of asbestos and products containing asbestos: for asbestos-cement products, with an annual production of more than 20 000 tonnes of finished products, for friction material, with an annual production of more than 50 tonnes of finished products, and for other uses of asbestos, utilization of more than 200 tonnes per year.
- Integrated chemical installations.
- Construction of motorways, express roads (1) and lines for long-distance railway traffic and of airports (2) with a basic runway length of 2 100 m or more.
- Trading ports and also inland waterways and ports for inland-waterway traffic which permit the passage of vessels of over 1 350 tonnes.
- Waste-disposal installations for the incineration, chemical treatment or land fill of toxic and dangerous wastes.
- Groundwater abstraction or artificial groundwater recharge schemes where the annual volume of water abstracted or recharged is equivalent to or exceeds 10 million cubic metres.
- Waste water treatment plants with a capacity exceeding 150000 population equivalent as defined in point 6 of Article 2 of Council Directive 91/271/EEC of 21 May 1991 concerning urban waste-water treatment.
- Extraction of petroleum and natural gas for commercial purposes where the amount extracted exceeds 500 tonnes/day in the case of petroleum and 500000 cubic metres/day in the case of gas.
- Dams and other installations designed for the holding back or permanent storage of water, where a new or additional amount of water held back or stored exceeds 10 million cubic metres.
- Installations for the intensive rearing of poultry or pigs with more than: (a) 85000 places for broilers, 60000 places for hens; (b) 3000 places for production pigs (over 30 kg); or (c) 900 places for sows.
- Industrial plants for the production of: (a) pulp from timber or similar fibrous materials; (b) paper and board with a production capacity exceeding 200 tonnes per day.

- Quarries and open-cast mining where the surface of the site exceeds 25 hectares, or peat extraction, where the surface of the site exceeds 150 hectares.
- 20. Construction of overhead electrical power lines with a voltage of 220 kV or more and a length of more than 15 km.
- Installations for storage of petroleum, petrochemical, or chemical products with a capacity of 200000 tonnes or more.
- Storage sites pursuant to Directive 2009/31/EC of the European Parliament and of the Council of 23 April 2009 on the geological storage of carbon dioxide.
- Installations for the capture of CO<sub>2</sub> streams for the purposes of geological storage pursuant to Directive 2009/31/EC from installations covered by this Annex, or where the total yearly capture of CO<sub>2</sub> is 1,5 megatonnes or more.
- 24. Any change to or extension of projects listed in this Annex where such a change or extension in itself meets the thresholds, if any, set out in this Annex.
- Pipelines with a diameter of more than 800 mm and a length of more than 40 km:

Notes: -

Nuclear power stations and other nuclear reactors cease to be such an installation when all nuclear fuel and other radioactively contaminated elements have been removed permanently from the installation site.

For the purposes of the Directive, 'express road' means a road which complies with the definition in the European Agreement on main international traffic arteries of 15 November 1975.

For the purposes of this Directive, 'airport' means airports which comply with the definition in the 1944 Chicago Convention setting up the International Civil Aviation Organization (Annex 14).

## A.2. ANNEX II Projects Subject to Article 4(2) of the EIA Directive

1. Agriculture
  - a. Projects for the restructuring of rural land holdings.
  - b. Projects for the use of uncultivated land or semi-natural areas for intensive agricultural purposes.
  - c. Water-management projects for agriculture.
  - d. Initial afforestation where this may lead to adverse ecological changes and land reclamation for the purposes of conversion to another type of land use.
  - e. Poultry-rearing installations.
  - f. Pig-rearing installations.
  - g. Salmon breeding.
  - h. Reclamation of land from the sea.
2. Extractive industry
  - a. Extraction of peat.
  - b. Deep drillings with the exception of drillings for investigating the stability of the soil and in particular:
  - c. Geothermal drilling,
  - d. Drilling for the storage of nuclear waste material,
  - e. Drilling for water supplies.
  - f. Extraction of minerals other than metalliferous and energy-producing minerals, such as marble, sand, gravel, shale, salt, phosphates and potash.
  - g. Extraction of coal and lignite by underground mining.
  - h. Extraction of coal and lignite by open-cast mining.
  - i. Extraction of petroleum.
  - j. Extraction of natural gas.
  - k. Extraction of ores.
  - l. Extraction of bituminous shale.
  - m. Extraction of minerals other than metalliferous and energy-producing minerals by open-cast mining.
  - n. Surface industrial installations for the extraction of coal, petroleum, natural gas and ores, as well as bituminous shale.
  - o. Coke ovens (dry coal distillation).
  - p. Installations for the manufacture of cement.

3. Energy industry

- a. Industrial installations for the production of electricity, steam and hot water (unless included in Annex I).
- b. Industrial installations for carrying gas, steam and hot water; transmission of electrical energy by overhead cables.
- c. Surface storage of natural gas.
- d. Underground storage of combustible gases.
- e. Surface storage of fossil fuels.
- f. Industrial briquetting of coal and lignite.
- g. Installations for the production or enrichment of nuclear fuels.
- h. Installations for the reprocessing of irradiated nuclear fuels.
- i. Installations for the collection and processing of radioactive waste (unless included in Annex I).
- j. Installations for hydroelectric energy production.

4. Processing of metals

- a. Iron and steelworks, including foundries, forges, drawing plants and rolling mills (unless included in Annex I).
- b. Installations for the production, including smelting, refining, drawing and rolling, of nonferrous metals, excluding precious metals.
- c. Pressing, drawing and stamping of large castings.
- d. Surface treatment and coating of metals.
- e. Boilermaking, manufacture of reservoirs, tanks and other sheet-metal containers.
- f. Manufacture and assembly of motor vehicles and manufacture of motor-vehicle engines.
- g. Shipyards.
- h. Installations for the construction and repair of aircraft.
- i. Manufacture of railway equipment.
- j. Swaging by explosives.
- k. Installations for the roasting and sintering of metallic ores.

5. Mineral Industry

- a. Coke ovens (dry coal distillation).
- b. Installations for the manufacture of cement.
- c. Installations for the production of asbestos and the manufacture of asbestos products (projects not included in Annex I).

- d. Installations for the manufacture of glass including glass fibre.
  - e. Installations for smelting mineral substances including the production of mineral fibres.
  - f. Manufacture of ceramic products by burning, in particular roofing tiles, bricks, refractory bricks, tiles, stoneware or porcelain.
6. Chemical industry
- a. Treatment of intermediate products and production of chemicals (unless included in Annex I).
  - b. Production of pesticides and pharmaceutical products, paint and varnishes, elastomers and peroxides.
  - c. Storage facilities for petroleum, petrochemical and chemical products.
7. Food industry
- a. Manufacture of vegetable and animal oils and fats.
  - b. Packing and canning of animal and vegetable products.
  - c. Manufacture of dairy products.
  - d. Brewing and malting.
  - e. Confectionery and syrup manufacture.
  - f. Installations for the slaughter of animals.
  - g. Industrial starch manufacturing installations.
  - h. Fish-meal and fish-oil factories.
  - i. Sugar factories.
8. Textile, leather, wood and paper industries
- a. Wool scouring, degreasing and bleaching factories.
  - b. Manufacture of fibre board, particle board and plywood.
  - c. Manufacture of pulp, paper and board.
  - d. Fibre-dyeing factories.
  - e. Cellulose-processing and production installations.
  - f. Tannery and leather-dressing factories.
9. Rubber industry
- a. Manufacture and treatment of elastomer-based products.
10. Infrastructure projects
- a. Industrial-estate development projects.
  - b. Urban-development projects.

- c. Ski-lifts and cable-cars.
- d. Construction of roads, harbours, including fishing harbours, and airfields (projects not listed in Annex I).
- e. Canalization and flood-relief works.
- f. Dams and other installations designed to hold water or store it on a long-term basis.
- g. Tramways, elevated and underground railways, suspended lines or similar lines of a particular type, used exclusively or mainly for passenger transport.
- h. Oil and gas pipeline installations.
- i. Installation of long-distance aqueducts.
- j. Yacht marinas.

#### 11. Other projects

- a. Holiday villages, hotel complexes.
- b. Permanent racing and test tracks for cars and motor cycles.
- c. Installations for the disposal of industrial and domestic waste (unless included in Annex I).
- d. Waste water treatment plants.
- e. Sludge-deposition sites.
- f. Storage of scrap iron.
- g. Test benches for engines, turbines or reactors.
- h. Manufacture of artificial mineral fibres.
- i. Manufacture, packing, loading or placing in cartridges of gunpowder and explosives.
- j. Knackers' yards.

#### 12. Tourism and Leisure

- a. Ski runs, ski lifts and cable cars and associated developments.
- b. Marinas.
- c. Holiday villages and hotel complexes outside urban areas and associated developments.
- d. Permanent campsites and caravan sites.
- e. Theme parks.

#### 13. Modifications to development projects included in Annex I and projects in Annex I undertaken exclusively or mainly for the development and testing of new methods or products and not used for more than one year.

### A.3. ANNEX III Information Referred to in Article 5(1)

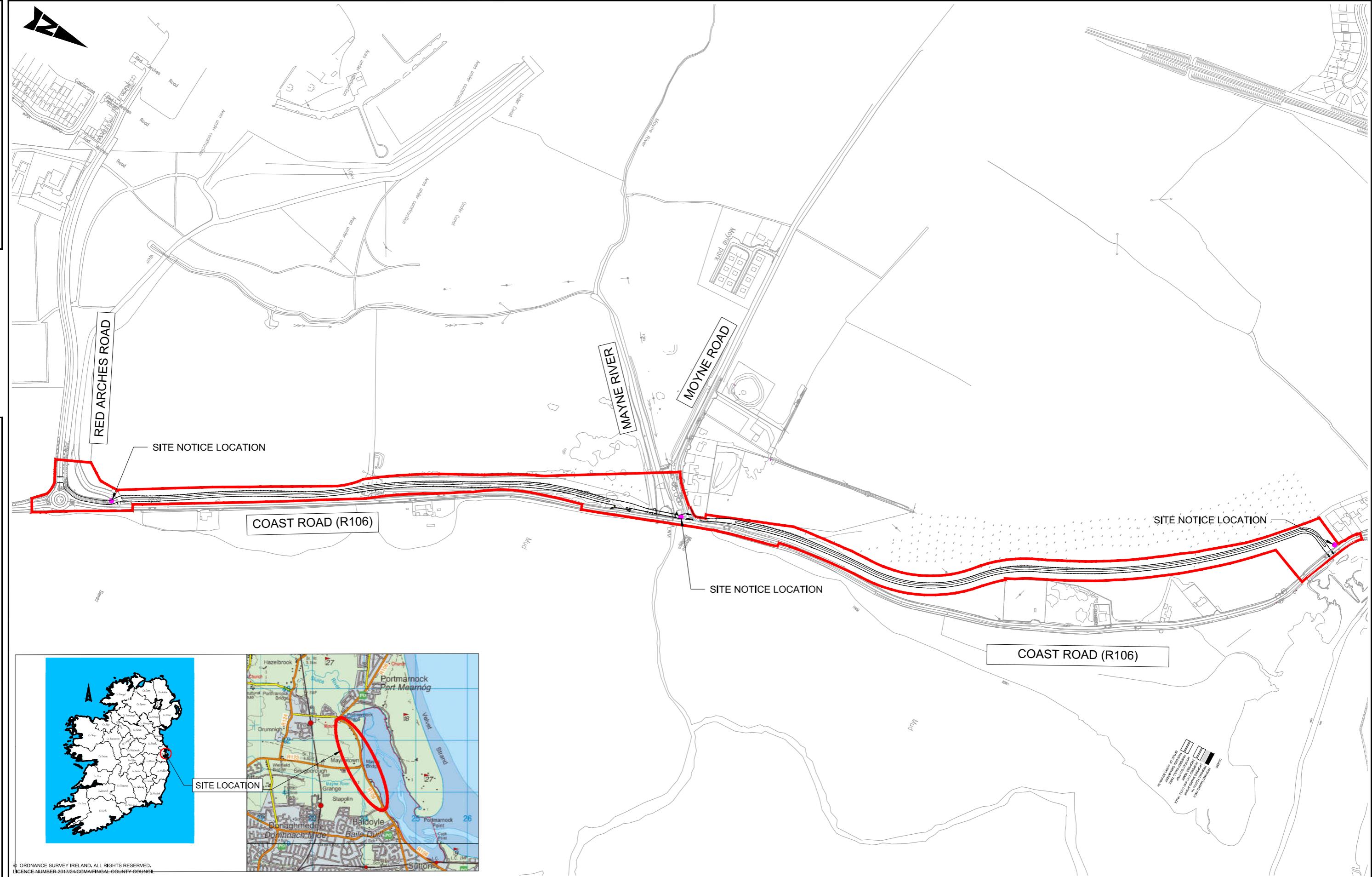
1. Description of the project, including in particular:
  - a description of the physical characteristics of the whole project and the land-use requirements during the construction and operational phases,
  - a description of the main characteristics of the production processes, for instance, nature and quantity of the materials used,
  - an estimate, by type and quantity, of expected residues and emissions (water, air and soil pollution, noise, vibration, light, heat, radiation, etc.) resulting from the operation of the proposed project.
2. Where appropriate, an outline of the main alternatives studied by the developer and an indication of the main reasons for his choice, taking into account the environmental effects.
1. A description of the aspects of the environment likely to be significantly affected by the proposed project, including, in particular, population, fauna, flora, soil, water, air, climatic factors, material assets, including the architectural and archaeological heritage, landscape and the inter-relationship between the above factors.
2. A description (1) of the likely significant effects of the proposed project on the environment resulting from:
  - the existence of the project,
  - the use of natural resources,
  - the emission of pollutants, the creation of nuisances and the elimination of waste;
  - and the description by the developer of the forecasting methods used to assess the effects on the environment.
3. A description of the measures envisaged to prevent, reduce and where possible offset any significant adverse effects on the environment.
4. A non-technical summary of the information provided under the above headings.
5. An indication of any difficulties (technical deficiencies or lack of know-how) encountered by the developer in compiling the required information.

(1) This description should cover the direct effects and any indirect, secondary, cumulative, short, medium and long-term, permanent and temporary, positive and negative effects of the project.

## Appendix B. Design Drawings

A1

DO NOT SCALE

Plotted by: swse  
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Date: Dec 13, 2017 - 23:28pm

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LICENCE NUMBER 204724/CCMA/FINGAL COUNTY COUNCIL

OS TILE REFERENCES:  
3065-D  
3066-C  
3133-B  
3134-A

KEY:



SITE LOCATION

- FOR INFORMATION BH 08.12.17 SW MD

Rev Description By Date Chkd Auth

**ATKINS**  
WS Atkins Ireland Ltd. ©

Atkins House, 150-155 Airside  
Business Park, Swords, Co. Dublin  
Tel (+353) 01 810 8000  
Fax (+353) 01 810 8001

Unit 2B, 2200 Cork Airport  
Business Park, Cork  
Tel (+353) 021 429 0300  
Fax (+353) 021 429 0360

Client  
**FINGAL COUNTY COUNCIL**

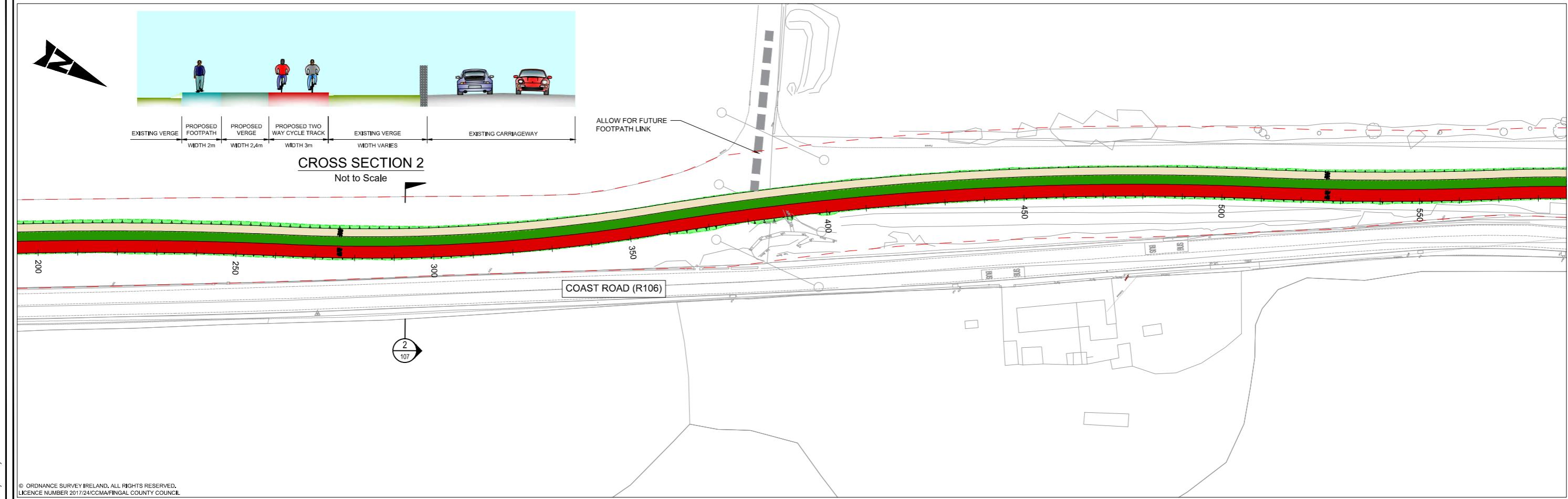
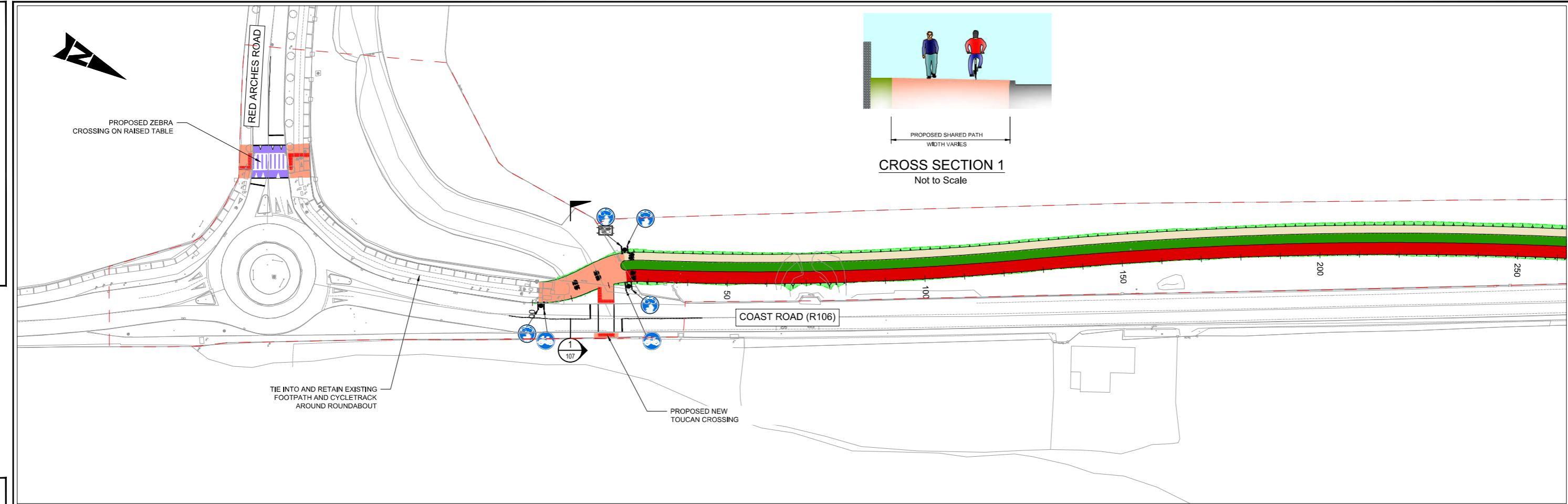
Project  
**BALDOYLE TO PORTMARNOCK  
WALKING AND CYCLING ROUTE**

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Date	08.12.17	Date	08.12.17	Date	08.12.17
Status	P	Drawing Number		Rev	-
	5158369 / HTR / SK / 0119				

A1

DO NOT SCALE

SW92

Plotted by:  
File: 5158369\_HTR\_SK\_0120.dwg  
Date: Dec 13, 2017 - 3:22pm© ORDNANCE SURVEY IRELAND. ALL RIGHTS RESERVED.  
LICENCE NUMBER 2017/24/CCMA/FINAL COUNTY COUNCIL

LEGEND
PROPOSED SHARED PATH
PROPOSED FOOTPATH
PROPOSED SHARED BRIDGE
PROPOSED TWO WAY CYCLE TRACK
PROPOSED VERGE
MODIFIED BUS STOP
PROPOSED RAISED TABLE
PROPOSED EMBANKMENT
EXTENT OF WORKS BOUNDARY



FOR INFORMATION	
Rev	BH 08.12.17 SW MD

Atkins House, 150-155 Airside  
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Tel (+353) 01 810 8000  
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Parkmore Technology Park, Galway  
Tel (+353) 091 786 050  
Fax (+353) 091 779 830

**ATKINS**  
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Client

FINGAL COUNTY COUNCIL

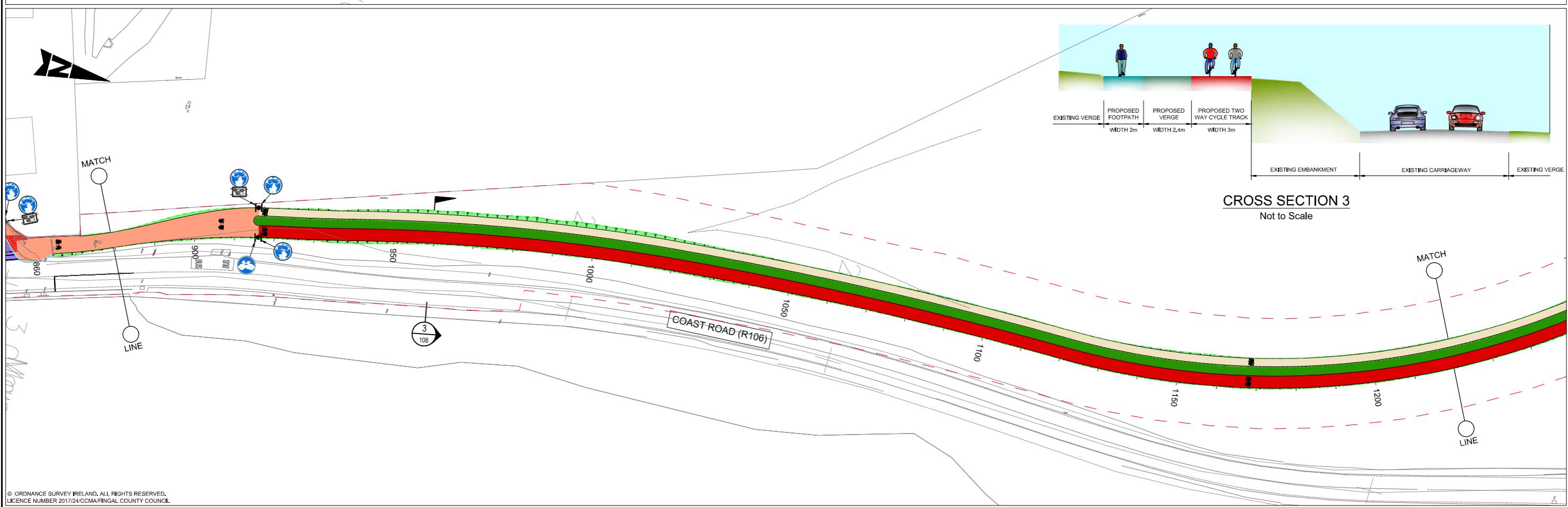
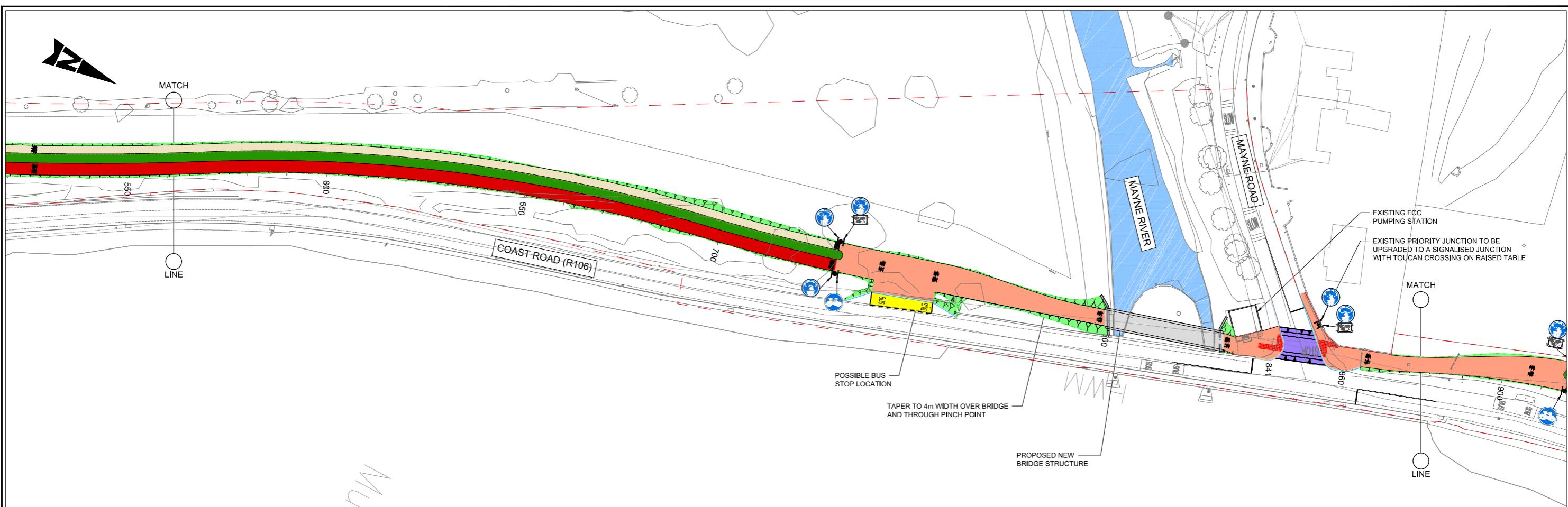
Project

BALDOYLE TO PORTMARNOCK  
WALKING AND CYCLING ROUTE

SITE LAYOUT PLAN SHEET 1 OF 3	
Original Scale 1:500 at A1 1:1000 at A3	Design Drawn BH Checked SW Authorised MD
Date 08.12.17	Date 08.12.17 Date 08.12.17 Date 08.12.17
Status P	Drawing Number 5158369 / HTR / SK / 0120 Rev -

A1

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LICENCE NUMBER 2017/24/CCMA/FINAL COUNTY COUNCIL

LEGEND	
PROPOSED SHARED PATH	
PROPOSED FOOTPATH	
PROPOSED SHARED BRIDGE	
PROPOSED TWO WAY CYCLE TRACK	
PROPOSED VERGE	
MODIFIED BUS STOP	
PROPOSED RAISED TABLE	
PROPOSED EMBANKMENT	
EXTENT OF WORKS BOUNDARY	



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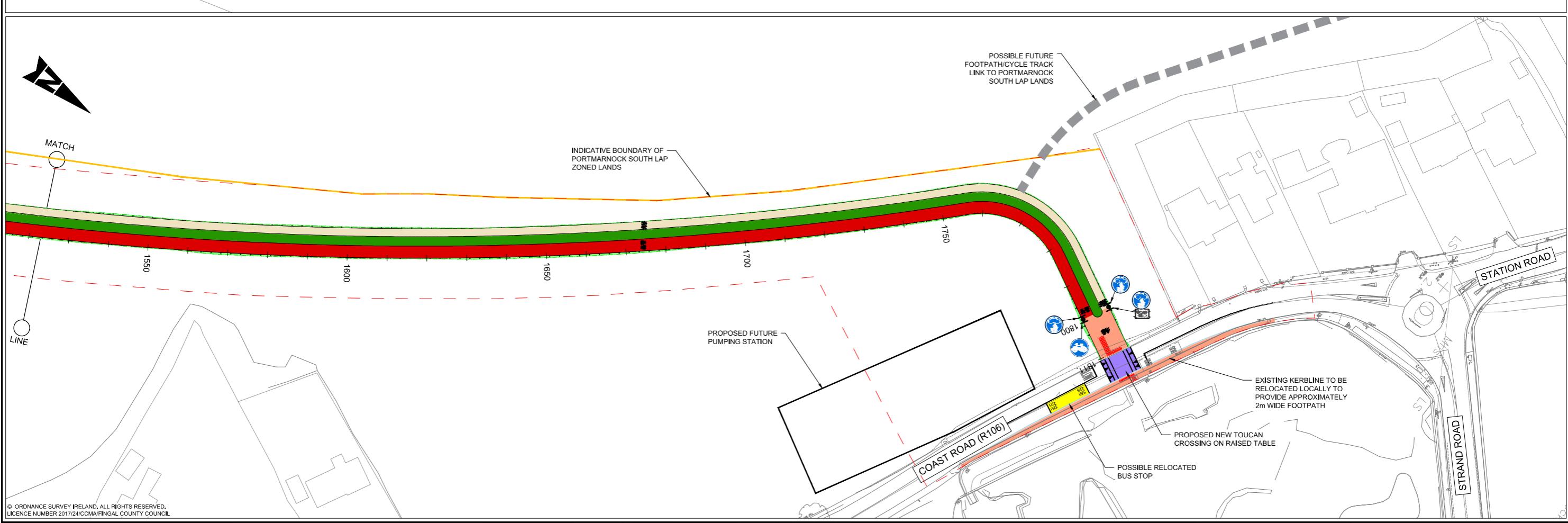
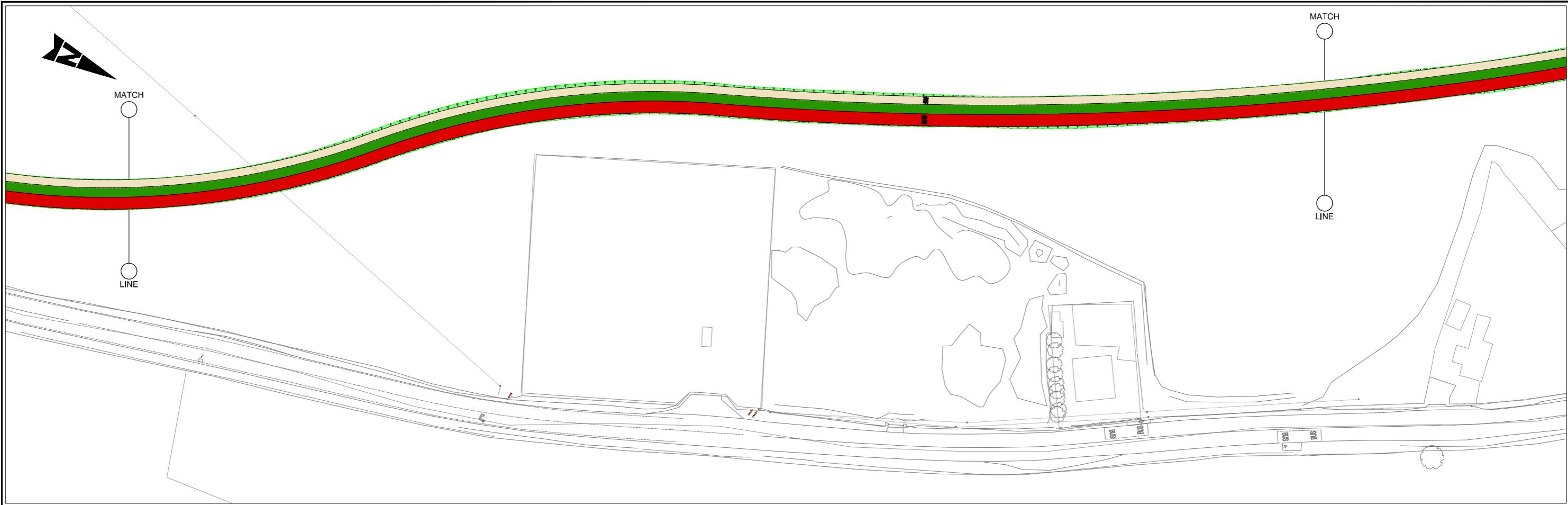
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Client  
**FINGAL COUNTY COUNCIL**  
Project  
**BALDOYLE TO PORTMARNOCK  
WALKING AND CYCLING ROUTE**

Title		SITE LAYOUT PLAN SHEET 2 OF 3			
Original Scale		Design/Drawn	Checked	SW	Authorised
1:500 at A1		BH			MD
		Date	08.12.17	Date	08.12.17
Status	Drawing Number				Rev
P	5158369 / HTR / SK / 0121				-

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LEGEND	
PROPOSED SHARED PATH	
PROPOSED FOOTPATH	
PROPOSED SHARED BRIDGE	
PROPOSED TWO WAY CYCLE TRACK	
PROPOSED VERGE	
MODIFIED BUS STOP	
PROPOSED RAISED TABLE	
PROPOSED EMBANKMENT	
EXTENT OF WORKS BOUNDARY	



FOR INFORMATION		BH	08.12.17	SW	MD
Rev	Description	By	Date	Chk'd	Auth

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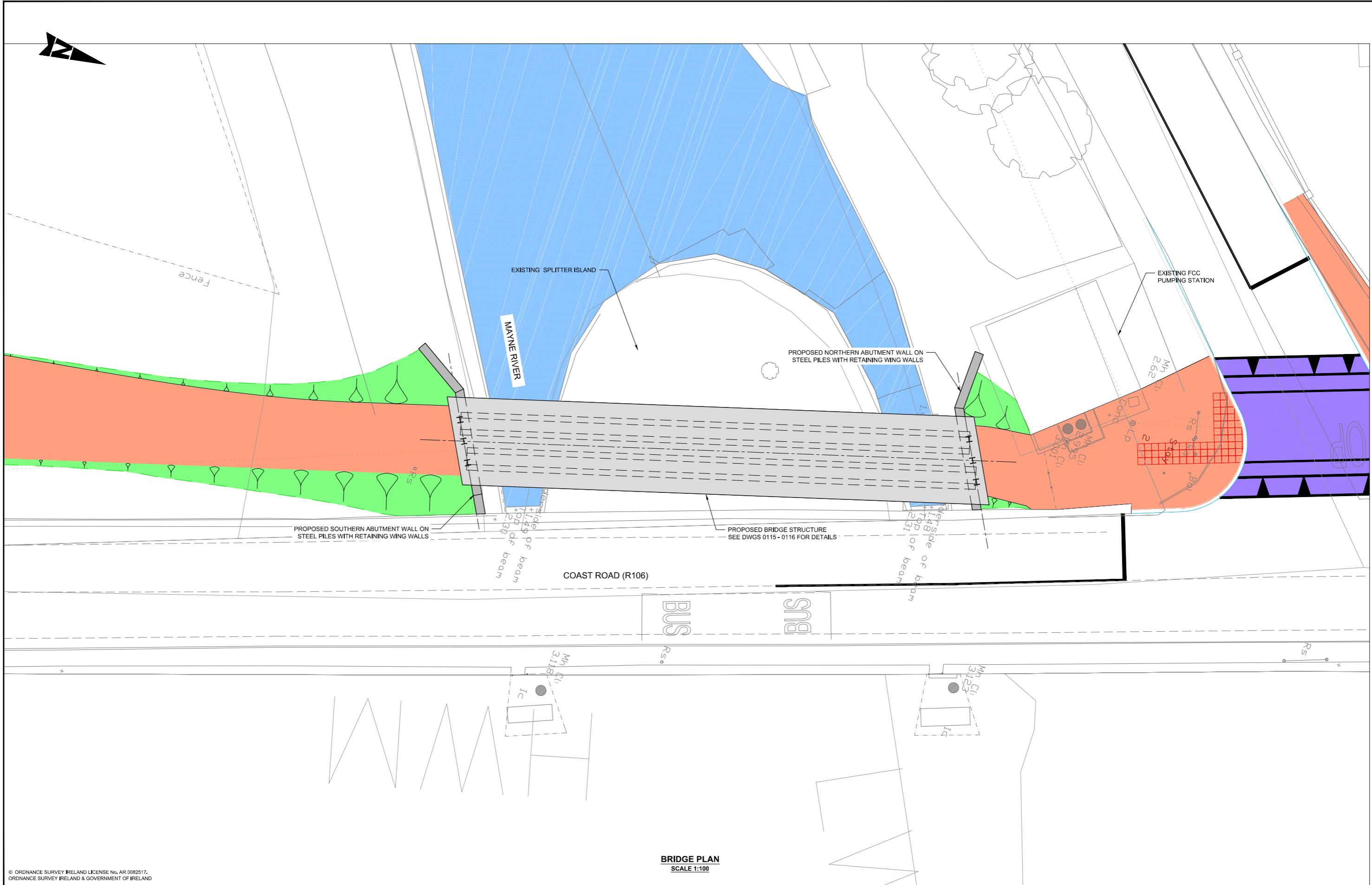
Client  
**FINGAL COUNTY COUNCIL**  
Project  
**BALDOYLE TO PORTMARNOCK  
WALKING AND CYCLING ROUTE**

Title		SITE LAYOUT PLAN SHEET 3 OF 3	Original Scale	Design/Drawn	Checked	Authorised
			1:500 at A1	BH	SW	MD
Project		1:1000 at A3	Date	08.12.17	Date	08.12.17
Status	Drawing Number	P	U:\5158369\6_Dwgs-Graphics\61_AutoCad\5158369_HTR_SK_0122.dwg	5158369 / HTR / SK / 0122	Rev	-

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BY SPACER

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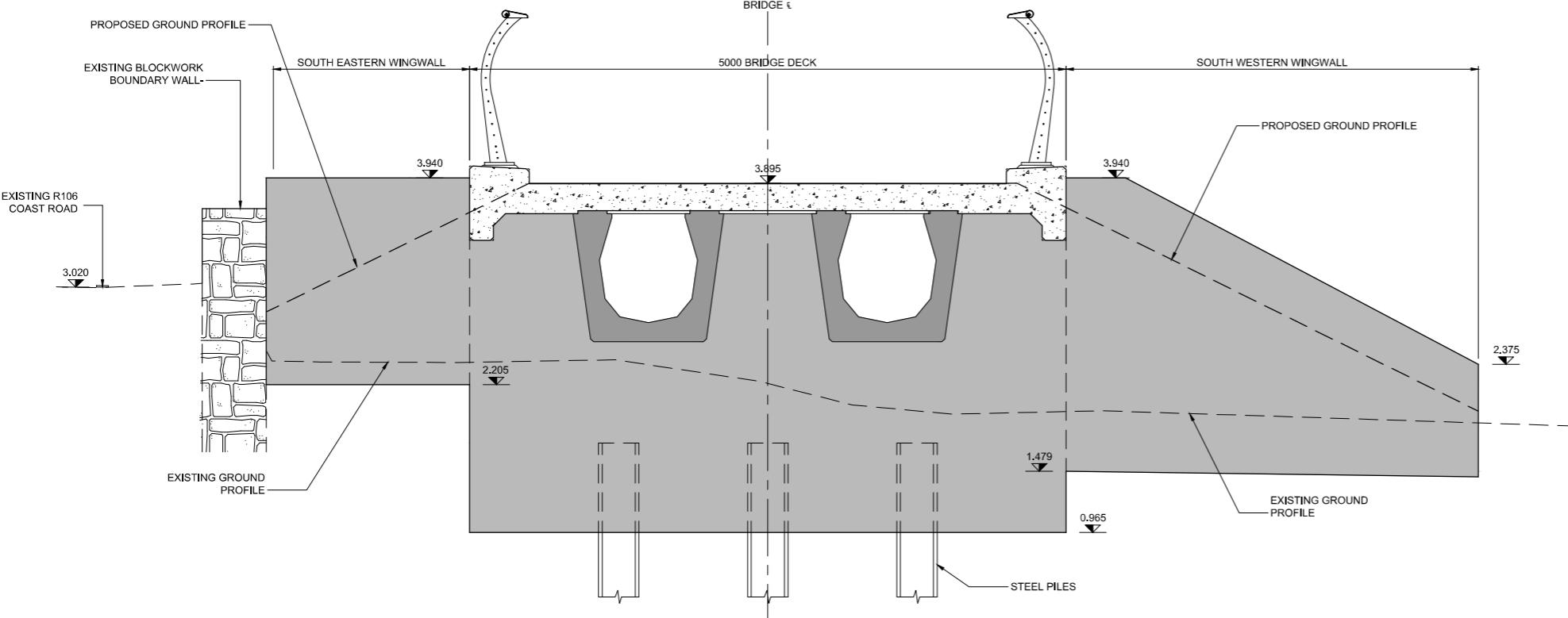
LEGEND									
PROPOSED SHARED PATH									
PROPOSED FOOTPATH									
PROPOSED SHARED BRIDGE									
PROPOSED TWO WAY CYCLE TRACK									
PROPOSED VERGE									
MODIFIED BUS STOP									
PROPOSED RAISED TABLE									
PROPOSED EMBANKMENT									

U:\5158369\6 Dwg-Graphics\61 AutoCad\5158369\_HTR\_SK\_0114.dwg

Rev	Description	By	Date	Chk'd	Auth	Atkins House, 150-155 Ainslie Business Park, Swords, Co. Dublin Tel (+353) 01 810 8000 Fax (+353) 01 810 8001	Unit 2B, 2200 Cork Airport Business Park, Cork Tel (+353) 021 429 0300 Fax (+353) 021 429 0360	2nd Floor Technology House Parkmore Technology Park, Galway Tel (+353) 091 786 050 Fax (+353) 091 779 830	Client FINGAL COUNTY COUNCIL	Title PROPOSED BRIDGE PLAN
									Project BALDOYLE TO PORTMARNOCK WALKING AND CYCLING ROUTE	
									Original Scale 1:100 1:200 Design/Drawn AOS Date 28/08/17 Checked SW Authorised MD Status Drawing Number I 5158369 / HTR / SK / 0114 Rev -	

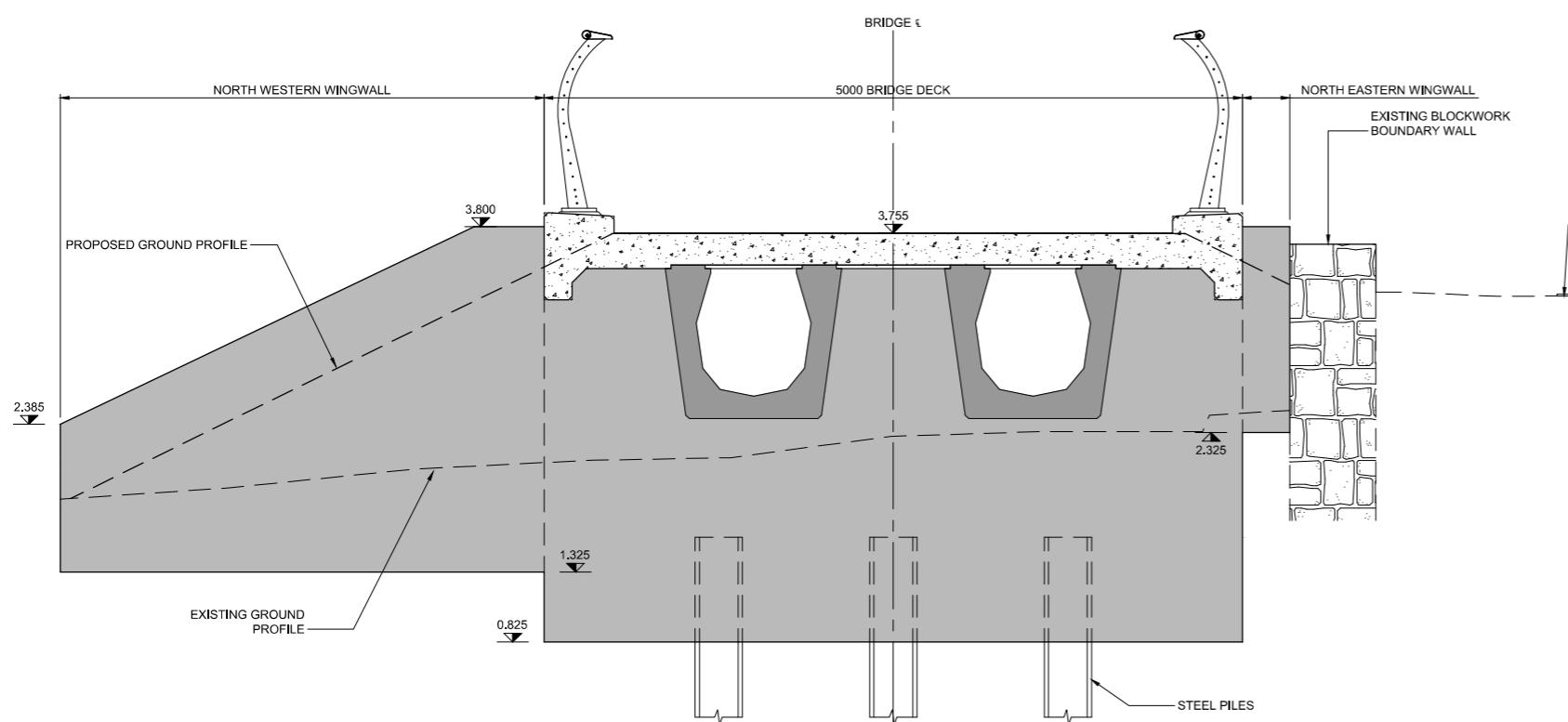
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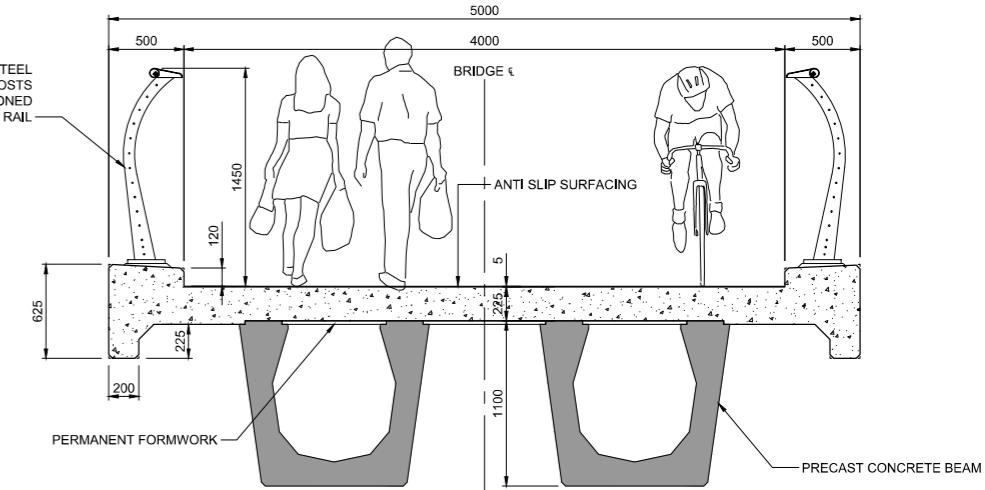
## ELEVATION - SOUTHERN BRIDGE ABUTMENT

Scale at A1 1:  
Scale at A3 1:



## ELEVATION - NORTHERN BRIDGE ABUTMENT

Scale at A1 1:  
Scale at A3 1:



## TYPICAL BRIDGE CROSS SECTION

Scale at A1 1:25  
Scale at A3 1:50

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File: 5158369\_HTR\_SK\_0115.dwg

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FINGAL COUNTY COUNCIL

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#### BALDOYLE TO PORTMARNOCK

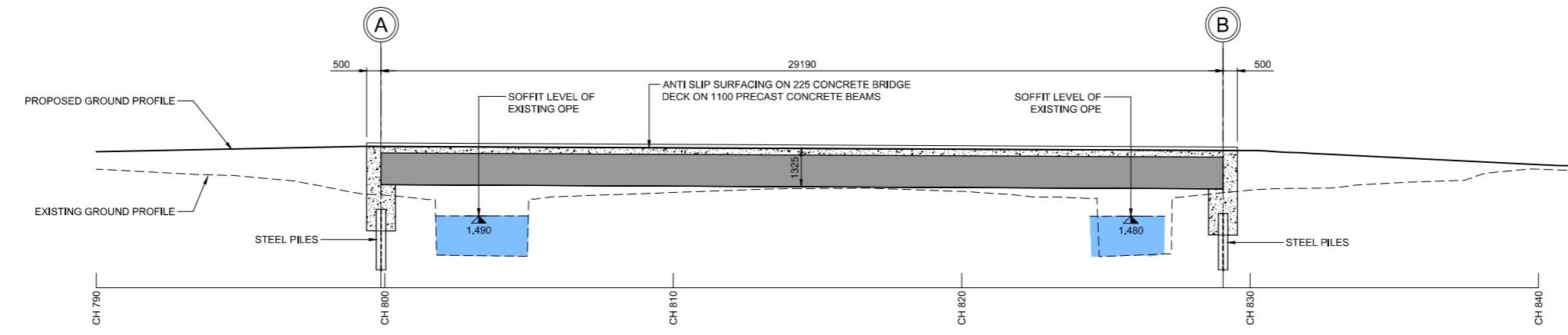
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		AOS	SW	MD	
Status	Drawing Number			Rev	
	5158369 / HTR / SK / 0115			-	

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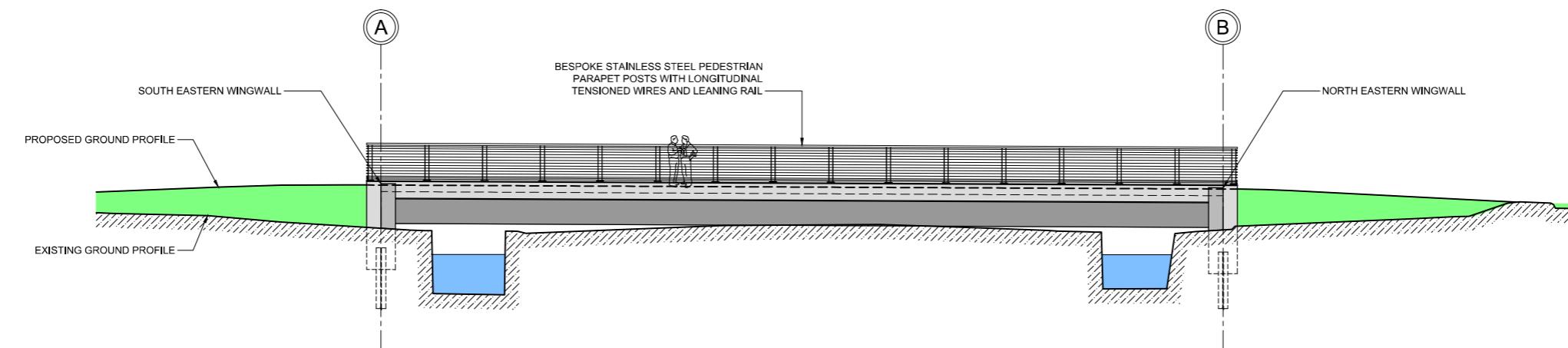
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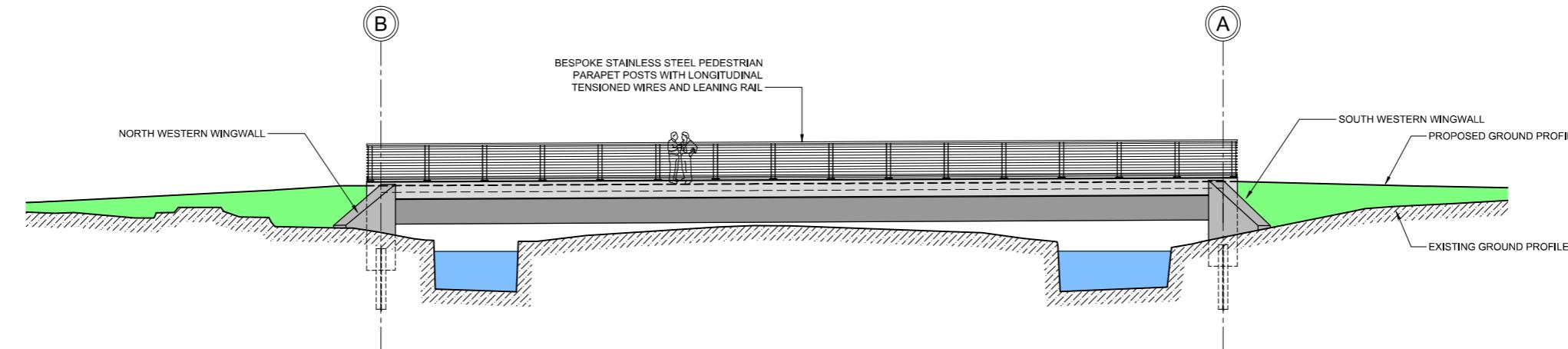
### TYPICAL LONGITUDINAL SECTION

Scale at A1 1:10  
Scale at A3 1:20



### EASTERN ELEVATION

Scale at A1 1:10  
Scale at A3 1:20



### WESTERN ELEVATION

Scale at A1 1:10  
Scale at A3 1:20



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## PROPOSED BRIDGE ELEVATIONS

X E	Original Scale		Design/Drawn	Checked	Authorised
			AOS	SW	MD
	Status	Drawing Number	Date	28/08/17	Date
	I	5158369 / HTR / SK / 0116			Rev -

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